

Myakka Valley Ranches

Improvement Association

ROADS AND TRAILS
MASTERPLAN

DRAFT

APRIL 2023



Background



Since the 1970's when Myakka Valley Ranches was developed, our valley has provided an escape from the hustle and bustle of city life, with a natural setting that allowed residents to live with nature through an equestrian life-style with ample outdoor recreational opportunities. Our valley is a unique community of residents who enjoy the slower pace of life with a connection to nature, through livestock, gardening, bee keeping, horseback riding, and many other hobbies.

In recent years, as a result of significant population growth in Sarasota County, new home construction has been encroaching from the west toward MVR, along the SR 72 (Clark Road) corridor. This growth has increased traffic and prompted road widening and two new roundabouts along Clark Road and a divergent diamond interchange at I-75.

As this development continues to push further eastward, our valley will face additional pressures. We hope this masterplan will serve as a framework to guide our actions to preserve this special place and our rural community values.



Acknowledgments



This Road and Trails Masterplan was prepared under the guidance and leadership of the following MVRIA members, with support from the community.

MVRIA Board (2022-23):

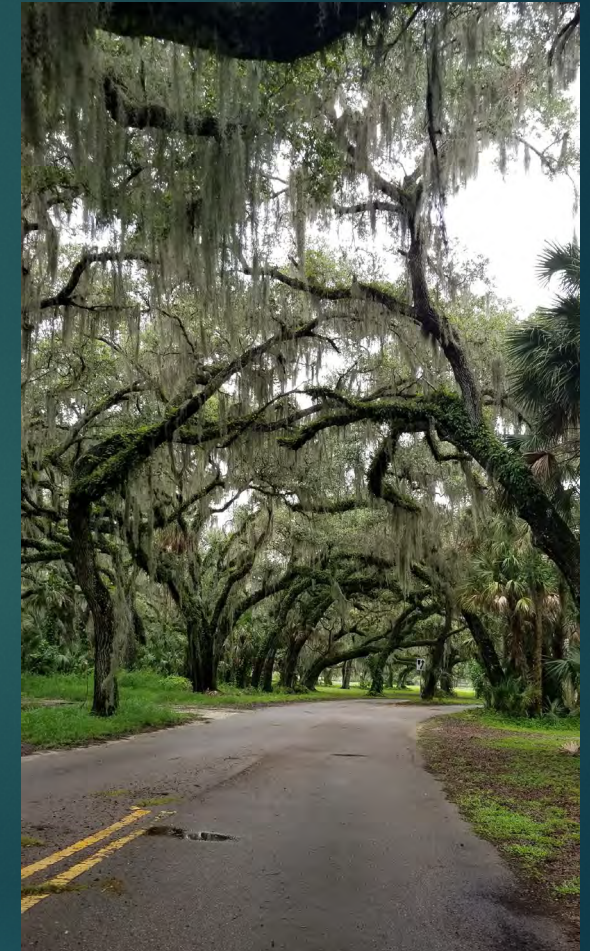
- Deb Houston - President
- Brenda Zimmerman - VP
- Jackie Vizzi – Secretary
- Sarah Leicht – Treasurer
- Jennifer Winkler
- Jim Bakowski
- Alice Strike
- Jason Bird
- Bryan Boley

MVRIA Roads Committee:

- Jennifer Peeples
- Jason Bird
- Amy Chenard
- Wendy DiGilio
- Melissa Fraley
- Deb Bakowski
- Dennis Perrault

MVRIA Trails Committee:

- Heather Nestle
- Amy Chenard
- Brenda Zimmerman
- Jason Bores
- Susan Gray
- Wendy DeGilio
- Wendy Ying



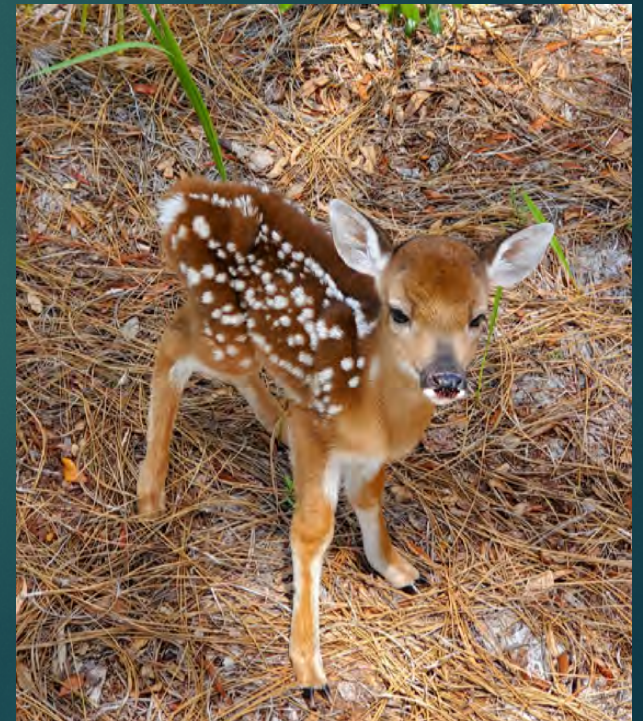
Executive Summary



This masterplan documents our community's vision, serving as the foundation for operational and capital decision making to guide development policy and community aesthetics to protect and promote our rural character, equestrian values and high quality of life.

This Master Plan supports these objectives through:

- Educating community members and leaders regarding the benefits of a masterplan.
- Establishing design standards that help to maintain the community rural aesthetics.
- Identifying priority actions for managing our built and natural resources.



Plan Contents



1. Preface

- a) Purpose
- b) How to use this Plan

2. Background

- a) Neighborhood Overview
- b) Improvement Association Responsibilities
- c) Utilities
- d) Natural Hazards & Other Challenges

3. Plan Development

- a) Community Engagement
- b) Process
- c) Plan Limitations

4. Community Vision and Guiding Principles

- a) Guiding Principles
- b) Vision Statement

5. Existing Conditions

- a) Road Pavement
- b) Road Signs
- c) Entrance
- d) Drainage
- e) Equestrian Trails & Parks
- f) Vegetation

6. Recommendations

- a) Maintenance Projects
- b) Capital Projects
- c) Project Prioritization and Approvals

7. Design Standards

- a) Aesthetics, Materials & Specifications

8. Emergency Response

9. Plan Implementation

10. References & Appendices

1.0

PREFACE



- a) Purpose
- b) How to use this Plan



Purpose

This Road & Trails Masterplan is intended to:

- ▶ Develop a vision for the roads, easements and open spaces in our neighborhood;
- ▶ Establish guidance for planning and resource management;
- ▶ Identify community needs and prioritize investment;
- ▶ Develop design standards for capital and maintenance projects;
- ▶ Establish legal standing for plan implementation.



How to Use This Plan

This plan should be referenced by the Neighborhood Committees and the Improvement Association Board to inform policy and improvement decisions to promote consistency and alignment with the community vision.

The primary Plan sections intended to guide these policy and improvement decisions include:

Section 4: Community Vision & Guiding Principles

Section 6: Recommended Projects

Section 7: Design Standards & Development Guidelines

Section 9: Plan Implementation

As neighborhood priorities, regulations and environmental conditions change, this plan should be updated to reflect these changes, to ensure the plan continues to support the intended outcomes as stated in the community vision.

2.0

BACKGROUND



- a) Neighborhood Overview
- b) Improvement Association Responsibilities
- c) Drainage & Utilities
- d) Natural Resources
- e) Natural Hazards & Other Challenges



Neighborhood Overview



History & Character

- ▶ Myakka Valley Ranches was platted in the 1970s as a rural equestrian residential subdivision with a minimum of 5-acre parcels and dedicated/platted equestrian trails and road and drainage easements, for the benefits of the association and its members.
- ▶ All parcels south of the nursery entrance, toward the front of the subdivision, are not platted or part of the association. Similarly, Westwood is also not part of the MVR association. However, since they share use of the common roads maintained by the association, they contribute to our road fund.
- ▶ The subdivision was built with rural roads (no curb and gutter) and open drainage channels to maintain the natural character and accommodate equestrian activities.

Tracts, Easements and Common Areas

- ▶ Since all property in MVR is private property, with no public ownership or rights of way, all easements are privately owned.
- ▶ Road easements are typically, 100 feet wide, with 50 feet dedicated from the property on either side of the shared property line, with a few narrow (50-75 ft) road easements. Drainage easements and saddle trail easements, typically ranging from 30-50 feet, with some as wide as 75 feet, are also equally divided on either side of a shared property line.
- ▶ A 160 ft wide Florida Power and Light easement also runs through the subdivision for overhead power transmission lines.
- ▶ There are also a few tracts of land along Howard Creek and within the floodplain, dedicated to the association.

Recreation & Natural Resources

- ▶ All association tracts and easements are maintained to preserve the natural resources and promote active and passive recreation, such as walking, horse riding and fishing.
- ▶ Motorized vehicles including 4-wheelers and dirt bikes are restricted to paved surfaces only within road easements and are not permitted on saddle trails or within association tracts.
- ▶ Residents in MVR cherish the rural character and preservation of the wildlife we have including deer, tortoises, birds of prey and many more.



Association Responsibilities

Overview

- ▶ The association is managed by a board of 9 directors with support from Progressive Community Management, Inc. (PCM), a hired attorney and a series of resident volunteer committees.

Bylaws

- ▶ The Association was incorporated as a non-profit in 1973, updated in 2008, has been maintained as such since that time.
- ▶ The bylaws define association membership and roles and responsibilities of board members and lays out the operational procedures for conducting business in compliance with state law.

Covenants

- ▶ Also known as Deed Restrictions or Declaration of Restrictions, last updated in 2008, describes the requirements of the association and members (lot owners) regarding maintenance of the association property, including allowable improvements and restrictions for private property and within easements.
- ▶ The covenants also lay out the process for collecting dues and for special assessments, when required.

Committees

- ▶ Compliance
- ▶ Dues
- ▶ Emergency Preparation
- ▶ Entrance Beautification
- ▶ Events
- ▶ Legal
- ▶ Roads
- ▶ Security
- ▶ Trails
- ▶ Website
- ▶ Welcome



Stormwater Drainage



Regional Setting

- ▶ MVR is located in a portion of the Lake Myakka watershed including conveyance of regional stormwater via Howard Creek, east and west branches.
- ▶ These creeks collect stormwater runoff from north and west of our subdivision and drain to Lake Myakka, which discharges via a weir and control structure, which regulates the water levels in the lake, to the south via the Myakka River, which discharges into Charlotte Harbor.
- ▶ The management of water levels in Lake Myakka and the stormwater flow conveyance within Howard Creek, managed by the County, is critical for preventing flooding in our neighborhood.

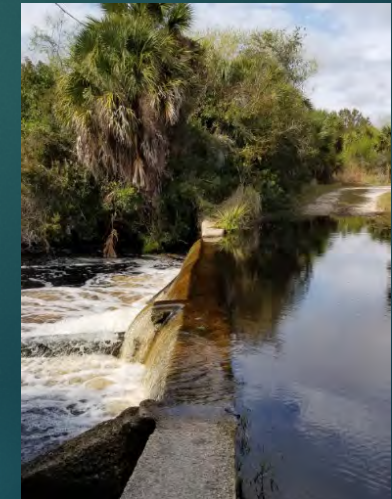


Sarasota County

- ▶ Through a maintenance agreement with the County, our major drainage channels and culverts are maintained, which helps to prevent flooding by keeping the conveyance free of vegetation or debris. This maintenance is performed on an annual basis during early summer using a Menzi Muck excavator.

Roadside Swales and Driveway Culverts

- ▶ Maintenance of the smaller roadside swales and driveway culverts are the joint responsibility of the association and homeowners to keep free of debris.
- ▶ The association is required to maintain drainage conveyance along association owned parcels, while private lot owners are responsible for maintaining their own swales and culverts.



Utilities



► Florida Power & Light (FPL)

Provides electric service via overhead distribution lines, fed from distribution lines along Clark Road (SR 72).

► Myakka Valley Communications

Provides high-speed internet service via buried fiber-optic lines. This locally owned and operated company originated as a result of the Connecting Rural America Initiative established in the early 2000's to provide grants and low interest loans to promote communications providers to service rural communities.



Communications equipment at MVT and Old Ranch Road.



Pole mounted electric transformer

► Frontier Communications

Provides phone and internet services via buried

► Sarasota County

Does not provide any utility services to MVR. However, does maintain major drainage channels to prevent blockage and ensure adequate capacity to minimize flood risk.

► Cable TV: Not available in MVR, except via satellite service providers.

► Natural Gas: Not available in MVR. However, buried liquid propane (LP) tanks are common.

► Potable Water: Private wells on each parcel. No public water supply service is available.

► Fire Water: No fire hydrants are provided in MVR; however, a low-pressure service connection is provided at the Fire Station, located at the front entry of the community, to fill tanker trucks for fire suppression.

► Reclaimed Water: Not available in MVR.

Natural Resources

Our Valley is located in a beautiful setting nestled against the Myakka State Park and surrounded by thousands of acres of forest and ranch land providing the perfect setting for abundant wildlife.

We are fortunate to live in this special place and share our backyards with the best nature has to offer, allowing us to observe wildlife, listen to water flowing down the creeks and take in beautiful sunrises and sunsets unobstructed by buildings or air pollution from the urban City setting.

If we manage our valley well, we will be able to continue enjoying these magnificent creatures and enjoy the natural environments we have protected allowing us to live in harmony with nature.



Natural Hazards & Other Challenges



► Flooding

It can flood anywhere it rains; however, most flooding occurs along creeks, drainage canals or adjacent to surface waters. Our Valley is bisected by Howard Creek, east and west branches flowing easterly, draining into Lake Myakka. The water levels in the lake are managed by an outfall control structure (weir) at the south end of the lake, drains into the Myakka River and toward Charlotte Harbor.

In addition to these regional conveyance creeks, flooding can also be caused by inadequate drainage such as along our roadside swales and through our driveway culverts, which is why it is so important to keep these drainage pathways clear and free of debris.



Rocking Horse Lane Flooding

► Electrical Power Outages

Loss of electric power has been an issue in MVR for many years as a result of the extensive tree canopy along the overhead power lines which has caused power faults when they come in contact by tree limbs. FPL has worked diligently over the years to improve the reliability of the service including tree trimming and system upgrades. Contact FPL at 1-800-468-8243 to report a power outage or to request tree trimming, at no cost to the resident, when tree limbs are close to the power lines.

► Cut Communication Lines

Cut communications lines has been an issue in the past, causing service interruptions, when utility locates are not performed prior to digging or construction including installing fence posts and planting trees/shrubs. Utility locates is a free service, usually performed within 1-2 days, by calling 811 or going to www.sunshine811.com to submit request.

► Speeding

The speed limit in the Valley is 30 mph, which is typical for a residential area, and 15 mph when equestrians are present. However, some residents and visitors/delivery drivers exceed this speed limit, posing a risk to our residents and wildlife in the Valley, requiring additional speed limit signs and other traffic calming devices.

Natural Hazards & Other Challenges



► Poaching, Vandalism and Theft

Our Valley is safe and very rarely experiences issues related to poaching, vandalism or theft. However, like any neighborhood, we occasionally hear of these activities, sometimes from kids or by outsiders who enter our Valley with ill intentions. Through our vigilance and communication with our neighbors, we can keep an eye out for suspicious activity and report it to authorities.

► Maintenance

All built infrastructure require maintenance to ensure they remain in good working order and perform their intended function. All assets have a service life, which can be shortened when not maintained, causing impacts to performance and requiring increased capital expenditures on an increased frequency. An asset condition assessment and maintenance plan is a critical tool.

► Mosquitos

[Sarasota County](#) provides spraying services, with an online map and information. You can request services at your home or register for spray notifications.



► Lack of Funding

The association is managed by the board using annual dues to cover operating expenses such as insurance, management company fees, attorney fees, community electric for the front entry, maintenance fees such as mowing common areas, and capital expenses, such as the allocated funding for road paving, as outlined in the annual budget.

The board has strived to keep the fees low by managing the costs and using volunteers to perform maintenance; however, this leaves little funding for larger projects, requiring our community to be frugal and stretch every dollar. The lawsuits in recent years have added an additional burden on our community, further straining our ability to maintain the roads.

► Community Engagement and Communications

We use multiple methods of reaching all residents in our valley to communicate important information and special events. These include, the monthly Tails and Trails newsletter, the MVR website, and direct mailings.

In addition, we have a membership directory that lists the lot number, address and phone number of all residents to help connect us with our neighbors, and some residents have created a Facebook page to share information, advertise eggs for sale or notify the neighbors of a missing dog.

3.0

PLAN DEVELOPMENT



- a) Community Engagement
- b) Process
- c) Plan Limitations



Community Engagement

- ▶ Monthly board meetings, newsletter and website
- ▶ Planned workshops (in-person and virtual)
- ▶ Review and comment on draft plan
- ▶ Annual meeting (road committee survey)



Process

- ▶ Step 1: Community visioning
- ▶ Step 2: Data collection and inventory (review of previous/existing plans & assets)
- ▶ Step 3: Data analysis: evaluating community needs based on vision statements
- ▶ Step 4: Preparing and adopting a plan (consider applicable laws and regulations)
- ▶ Step 5: Implementing the plan
- ▶ Step 6: Monitoring and updating the plan



2022 Annual meeting at St. Margaret's Episcopal Church

Plan Limitations



Not regulatory (state or county)

- ▶ This plan is not required by state or county policy, nor it is intended to ensure compliance with state and county requirements.

Changes in association board members

- ▶ As elected officials, representing the community, the association board make decisions that best serve the community and its current needs.
- ▶ As such, changes in board members may drive a different direction for the community and require updates to this plan.

**AMENDED AND RESTATED
DECLARATION OF RESTRICTIONS
FOR
MYAKKA VALLEY RANCHES**

WHEREAS, the Myakka Valley Ranches subdivision is composed of residential lots described in five recorded plats, the restrictions of each having been recorded in the Public Records of Sarasota County, Florida, and having been amended on occasion, as reflected in instruments recorded in the Public Records of Sarasota County, Florida, and

WHEREAS, the most recent restrictions for the subdivision were set forth in that certain Amended Declaration of Deed Restrictions recorded at Official Records Book 1862, Page 974, et seq., Public Records of Sarasota County, Florida, and

WHEREAS, in excess of fifty percent (50%) of the owners of record of the properties located in the Units 1, 2, 3, 4, and 5 approved these Amended and Restated Declaration of Restrictions.

NOW THEREFORE, MYAKKA VALLEY RANCHES IMPROVEMENT ASSOCIATION, INC. does hereby amend and restate the Declaration of Deed Restrictions for Myakka Valley Ranches for the purpose of integrating all of the provisions of this Declaration, together with previously recorded amendments, and recently adopted amendments, and does hereby resubmit the lands described herein to the terms, covenants, conditions, easements and restrictions hereof which shall be covenants running with the property and binding on all existing and future owners, and all others having an interest in the lands or occupying or using the property.

Evolving priorities

- ▶ This plan captures the vision and priorities of the community today, based on community leaders and resident input. However, over time these priorities can change; therefore, this plan should be updated from time to time to reflect these changes.
- ▶ Development pressure from future development from the west along Clark Road (SR 72) and Hi Hat Ranch and from the north via SRQ Rach may require a strong unified community with advocacy to protect our special rural way of life.
- ▶ These are examples of activities that may justify updates to this plan to ensure it reflects the current needs of the community.

4.0

COMMUNITY VISION & GUIDING PRINCIPLES



- a) Guiding Principles
- b) Vision Statement



Guiding Principles

The following guiding principles are based on community values that represent the interests of the current residents and board, listed in order of priority.

1. Community safety
2. Maintain property values
3. Protect our limited financial resources and avoid special assessments
4. Maintain reliable utility services (electric and communications)
5. Protect our equestrian and other recreational amenities
6. Protect wildlife and other natural resources
7. Minimize property flood risk or damage resulting from flood events
8. Maintain rural community aesthetic



Vision Statement

- Protect and promote our community's rural character, equestrian values and high quality of life through consistent application of supporting policies and capital expenditures.



5.0

EXISTING CONDITIONS

- a) Road Pavement
- b) Road Signs
- c) Entrance
- d) Drainage
- e) Equestrian Trails & Parks
- f) Vegetation



Road Pavement



- ▶ The roads in MVR are asphalt pavement over limestone base configured as a rural road section. Most roads consist of 20 feet of paved surface with a normal crown, grass shoulders and open drainage swales; however, a few short dead-end roads are narrower such as Kickapoo, Hupa and Papago.
- ▶ Asphalt roads have an approximate 30-40 year service life, requiring resurfacing about every 15-20 years, depending on conditions. The roads throughout the valley are in need of resurfacing, which is evidenced by the excessive surface cracking and pot holes, resulting from saturation of the limestone base from high groundwater and stormwater runoff.
- ▶ In 2010, the community began reserving 50% of the annual dues for a road pavement fund. These funds were used in 2019/20 to hire Gator Paving, Inc. to resurface the first phase of this plan, consisting of Myakka Valley Trail from SR 72 to O Bar Rd, and a portion of Old Ranch Road, for a total of 4 miles of roadway. The plan consists of additional phases to complete the remaining 10 miles of paving, when funds are available.
- ▶ The resurfacing consisted of repairing pot holes and damaged areas of pavement, then paving a 1.5 inch lift of asphalt on top of the existing pavement surface. Milling is used at the ends of the paving to create a smooth transition to the existing pavement to remain, until the next phase is completed.
- ▶ Following the current plan, it may take over 10 years to complete the resurfacing of all roads in the valley. However, other options are being explored to help fund/finance, such as low interest loans or grants to assist with expediting this project.



Road Signs



Overview

- ▶ As a private community the roads and road signs in MRV are owned and maintained by the association and are not required to follow the Sarasota County standards. However, following County standards and the Manual on Uniform Traffic Control Devices (MUTCD) (used by the County and FDOT) is a recommended best practice, where practicable to promote vehicular safety for residents and visitors.

Traffic Control

- ▶ At the request of numerous residents, approximately 20 new speed limit signs were installed throughout the valley intended to help reduce speeding.
- ▶ Speedbumps were also installed in two locations on MVT, as a result of a previous community vote, however, were not well received due to the loud noise created when vehicles drove over them, the concerns for injury to horses when driving over with horse trailer, and their general ineffectiveness in slowing traffic.



New road signs installed in 2021

Sign Inventory & Replacement

- ▶ In 2021, a road sign inventory, cleaning and condition assessment was performed to identify signs requiring repair or replacement.
- ▶ MVR has approximately 140 road signs, consisting of:
 - ▶ 24 stop signs
 - ▶ 22 speed limit signs
 - ▶ 50 directional or warning signs
 - ▶ 8 equestrian signs
 - ▶ 35 road name signs
- ▶ The results of the condition assessment resulted in replacement of approximately 20 directional and warning signs, the addition of equestrian signs, and wildlife crossing signs, the addition of 20 speed limit signs and replacement of approximately 20 road name signs.



Old road signs replaced in 2021.

Entrance

- ▶ The entry drive from SR 72 to our entry sign bisects part of the Myakka State Park, via an access easement.
- ▶ This access road is guarded by two old growth oak trees and a decorative stone and timber sign and lined with oak trees installed prior to 2018 with landscape lighting that welcomes residents and visitors.
- ▶ Our entry sign hangs from a milled timber archway/gantry with a small unmanned guardhouse with security cameras, located in a landscaped median.



Gate

- ▶ There has been a lot of discussion in recent years regarding the possibility of installing an entry gate to restrict access to MVR to residents and invited guests.
- ▶ This is a polarizing project with a deep divide between those residents who want the gate to improve security with the hopes of preventing vandalism or theft, and those residents who do not want to be inconvenienced by the gate, which will likely slow down access to the neighborhood.
- ▶ Before any decision is made, gate options with pros, cons and costs will be prepared and presented to the community for a vote.



Drainage

- ▶ The open drainage channels throughout the valley is a low maintenance approach to convey large quantities of stormwater to Howard Creek; however, these conveyance systems are limited by the over 300 culverts in roadside drainage swales and driveways. When culverts are blocked by debris or sediment, stormwater can flow over driveways and roads causing erosion and flooding, restricting access.
- ▶ Other challenges include ruts caused by ATVs and dirt bikes that disturb vegetation within the road easements and around drainage swales promoting erosion, which can increase sedimentation of drainage culverts and swales, and reduce their capacity to convey stormwater, resulting in flooding.
- ▶ Management of debris and sediment and maintaining drainage conveyance capacity is critical to reducing flooding and associated damage to our roads, homes and other assets.
- ▶ While the driveway culverts are the responsibility of the lot owners, and the road culverts are the responsibility of the Association, Sarasota County manages the platted drainage ditches in our neighborhood, and the South Florida Water Management District is responsible for Howard Creek.



Myakka Valley Trail - erosion



Myakka Valley Trail



Sheps Island Road

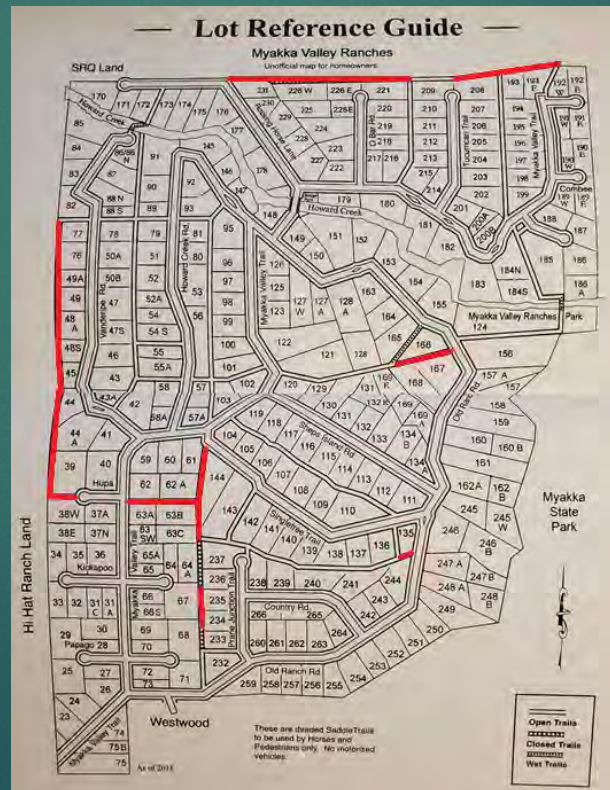


Rocking Horse Lane – riverine flooding

Equestrian Trails



- ▶ Saddle trail easements across 29 lots have been closed off or impassible due to erosion, vegetation, fencing or flooding.
- ▶ According to the recorded plat easement dedication, road and drainage easements are reserved for resident and equestrian use, but many are not passible/usable in their current state.
- ▶ EASEMENT NOTE (plat easement dedication): *Road and drainage easements are also reserved for use as saddle trails.*
- ▶ Trails may be closed for a variety of reasons including platted trails that were never cleared or opened, drainage ditches that block trails, excessive vegetation and flooding, or where property owners have blocked trails with fencing.



Physically Blocked Trails (red lines)



Chimney Trail near Howard Creek Road

Community Owned Property & Parks



- ▶ As shown on the recorded plat for MVR, there are several tracks dedicated to the Association for drainage, which allow recreational use by residents. These include:
 - ▶ Howard Creek Drainage Tract A (17.7 acres of floodplain and river)
 - ▶ Howard Creek Drainage Tract B (16.5 acres of floodplain and river)
 - ▶ Old Ranch Road Tract A (3.3 acres, road and shoulder at entrance to Westwood)
 - ▶ Lot 94 (6.0 acre drainage area encompassing the bridge and Howard Creek floodplain)
 - ▶ Lot 124 (26.7 acre drainage area, Old Ranch Road park, access to Howard Creek)



Lot 124 Community Park access gate



Lot 94 Community Park access gate



Vegetation



- ▶ Through preservation of our rural neighborhood character, our roads and trails are lined with large growth oak trees, cabbage palm trees and palmettos.
- ▶ This abundant vegetation requires maintenance to prevent from obstructing vehicular access, equestrian trails, and visibility at intersections, which can lead to unsafe conditions for vehicles, equestrians, pedestrians and bicyclists. Similarly, it is also important to maintain vegetation within drainage swales to prevent clogging.
- ▶ According to our deed restrictions, this maintenance responsibility falls on the private property owners to maintain the easements within each of their properties.



Sheps Island Road



Myakka Valley Trail, north of bridge



Drainage Swale along Myakka Valley Trail

6.0

RECOMMENDATIONS



- a) Maintenance Projects
- b) Equestrian Trails
- c) Capital Projects
- d) Project Costs
- e) Road Paving Project
- f) Project Prioritization and Approvals



Maintenance Projects



Maintenance projects are defined as projects to repair or replace existing infrastructure to maintain their intended function. These projects are generally lower cost projects that are planned for in the annual budget as a general expense, such as trail maintenance, pothole repair or tree trimming. Many of these projects can also be performed by community volunteers, or in collaboration with property owners (for work in easements). The following maintenance projects are in progress or proposed:

- ▶ **Road sign repair and replacement**

A condition assessment and replacement of damaged or missing road signs was completed in 2021-22, using the dedicated road maintenance fund.

- ▶ **Road pothole repair**

Resulting from saturated groundwater conditions during the rainy season, potholes are repaired with asphalt patch when discovered, using available funds from the dedicated road maintenance fund, performed by volunteer members of the road committee.

- ▶ **Road easement vegetation/tree trimming**

This ongoing activity requires coordination between private property owners and the association to maintain safe conditions along our roads, ideally using volunteers to perform the work, to reduce association costs.

- ▶ **Drainage culvert cleaning**

Maintaining unobstructed drainage culverts along roads under driveways is an ongoing activity that requires coordination between private property owners to prevent flooding and road/driveway damage, ideally using volunteers to perform the work, to reduce costs.

- ▶ **Fertilize oak trees at front entry road**

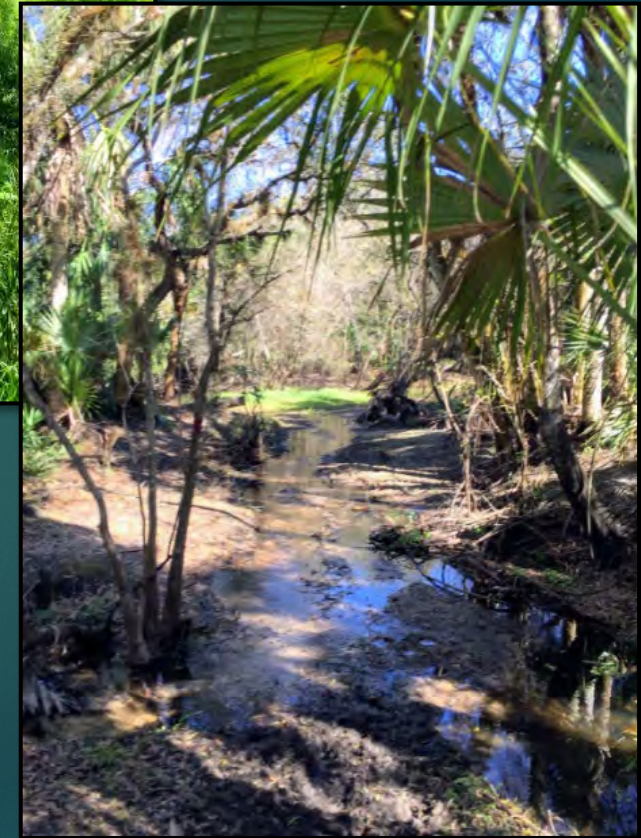
Annual deep root fertilizing will promote healthy trees and improved entry aesthetics.



Erosion at Creek drainage culvert

Equestrian Trail Projects

- Per HOA deed restrictions, all homeowners with obstructed saddle trails should be notified to open them within 30 days.
- All lots with closed trails should be reported to PCM, so that homes cannot complete closings if sold until they are in compliance with the deed restrictions and trails are opened.
- A budget should be included in the annual plan for maintaining overgrown front road easements.



Capital Projects



Capital projects are defined as projects that require a sizable investment, requiring advanced planning, budgeting and in many cases, a community vote. These projects usually require a contractor to perform the work. The following capital projects are in progress or proposed:

► Road paving

Phase 1 of the road repaving project was completed in 2019/20 with future phases planned when funding is available, via the dedicated road fund, paid for via annual member dues. Approximately 4 miles of road paving is complete, with 10 miles remaining. The rising cost of asphalt and labor has driven the cost of this project higher, requiring updated quotes from multiple contractors.

► Traffic calming (speed mitigation)

Various devices are available and require evaluation for application in our community to achieve the desired objectives without unintended consequences. The devices, which may include speed tables, islands, or radar speed signs, should be presented to the community to solicit input and to promote alignment, prior to implementing the projects.

► Entry gate

Restrictions such as the fire station access, and options for a gate at the MVR entrance need to be explored and presented to the community for a vote, prior to implementation, due to the high cost of this project and since it will affect all residents in the valley including Westwood and unrecorded parcels. Gate considerations include a dual entry lane and coded entry vs unrestricted gates.

► Drainage improvements

A few roads in our neighborhood flood frequently during rainy season as a result of inadequate or blocked drainage, affecting access and degrading the roads. Improvements to these areas is recommended, requiring further exploration.



Flooding across Myakka Valley Trail, north of bridge

Project Costs



Project Name (Maintenance Projects)	Estimated Cost	Description
Road sign repair and replacement	\$4,200 est. total	Project was completed in 2022.
Road pothole repair	\$18/40 lb bag of asphalt patch	Ongoing, as needed. Performed by resident volunteers.
Road easement vegetation/ tree trimming	\$0	Work performed by resident volunteers.
Drainage culvert cleaning	\$0	Work performed by resident volunteers. Any costs associated with culvert jetting or replacement will be incurred by property owner, with exceptions, decided by board.
Fertilizing oak trees at entry road	\$150	Annual fertilizing will help these trees to be healthy

Project Name (Capital Projects)	Estimated Cost	Description
Road paving	TBD, varies with timing	Remaining 10 miles of road to be prioritized based on condition and traffic volumes, price varies with price of crude oil.
Traffic calming (speed mitigation)	TBD, varies with approach	Road committee to explore these options and present pros/cons and costs to board for consideration.
Entry gate	TBD, varies with options	Road committee to explore these options and present pros/cons and costs to board for consideration.
Drainage improvements	TBD, varies with needs	Road committee to explore these options and present pros/cons and costs to board for consideration.

Road Paving Project (Phase 1)



The Road Paving project was Initiated in 2010 by a community vote to create a road reserve fund to dedicate 50% of the community annual dues toward road paving and supporting projects, which would be phased and completed as funds became available.

In 2020 phase 1 of the road raising project was completed with approximately 4 miles of road being paved, including MVT from Clark Road to O Bar Road, and along Old Ranch Road, from MVT (south intersection) to Prairie Junction Trail, as shown in Figure 1 below.

The Phase 1 paving represents approximately 27% of the 14.6 miles of roads in the valley.

The project was performed by Gator Paving and included the following activities:

- ▶ Paving preparations including pothole and damaged pavement repair, grinding existing pavement for transitions.
- ▶ Drainage culvert cleaning (at key areas to avoid possible drainage damage to new pavement)
- ▶ Paved apron at all cross streets.

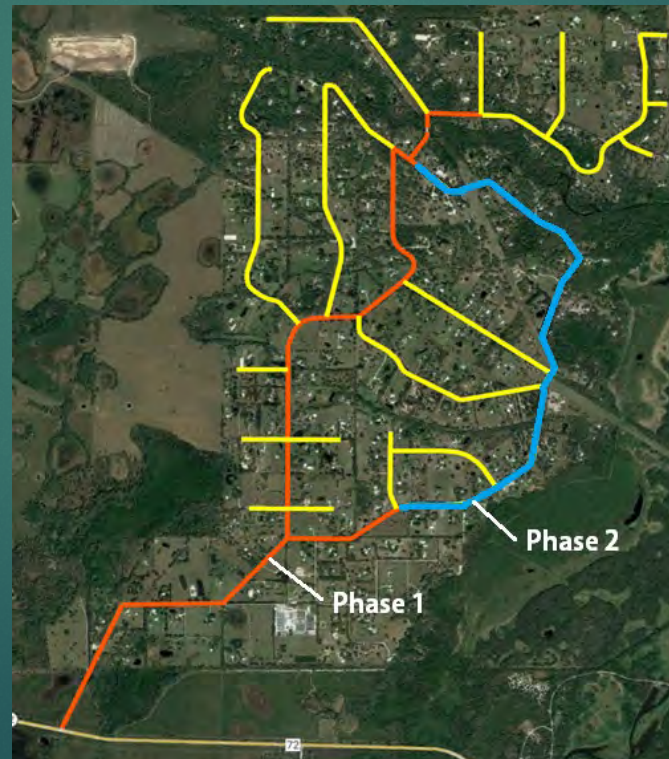


Figure 1: Road Paving in 2020 (Orange line)

MVRJA - Road Lengths			
Sept. 2021	Road Length (miles)		
Road Name	Paved in 2020	Remaining to Pave	Total Length
South of Bridge			
Myakka Valley Trail (south of bridge)	3.29	0.00	3.29
Old Ranch Road	0.45	2.12	2.57
Howard Creek Road	0.00	1.26	1.26
Vanderipe Road	0.00	1.15	1.15
Sheps Island Road	0.00	0.64	0.64
Singletree Trail	0.00	0.84	0.84
Country Road	0.00	0.45	0.45
Prairie Junction Trail	0.00	0.30	0.30
Hupa Road	0.00	0.19	0.19
Kickapoo Road	0.00	0.37	0.37
Papago Road	0.00	0.31	0.31
SUB-TOTALS	3.74	7.63	11.37
	33%	67%	
North of Bridge			
Myakka Valley Trail (north of bridge)	0.26	1.20	1.46
Combee Lane	0.00	0.10	0.10
No Name Road	0.00	0.15	0.15
Tucumcari Trail	0.00	0.42	0.42
O Bar Road	0.00	0.32	0.32
Rocking Horse Lane	0.00	0.77	0.77
SUB-TOTALS	0.26	2.96	3.22
	8%	92%	
TOTALS	4.00	10.59	14.59
	27%	73%	100%

Road Paving Project (Future Phases)



Future phases of road paving will be prioritized based on traffic volumes (number of lots served) and road condition, as observed (cracking, potholes, etc.) Road paving phasing will also consider proximity to adjacent roads to reduce mobilization costs and costs related to tie ins to existing pavement.

Phase 2 will include the remainder of Old Ranch Road from Prairie Junction Trail to MVT, as shown in Figure 2 below. Phase 3 is anticipated to include the remainder of Myakka Valley Trail, north of the bridge and adjacent roads including O Bar, Tucumari Trail, Combe Lane and the no name road.

Timing for these phases will depend on various factors including:

- ▶ Cost of fuel (directly affects cost of asphalt pavement)
- ▶ Availability of contractors (current construction market high activity has affected availability)
- ▶ Available funds in the road reserve fund account (alignment with project cost and available funds)

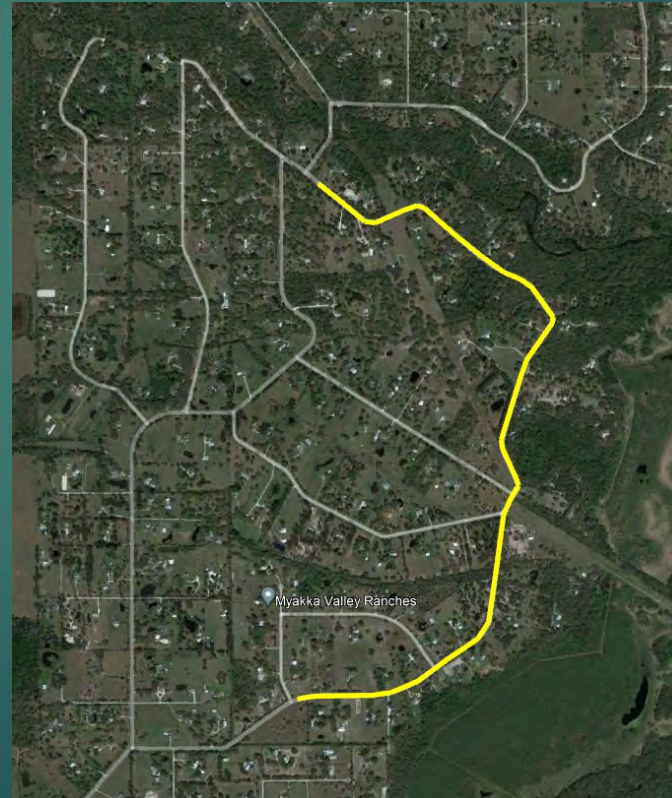


Figure 2: Road Paving Phase 2

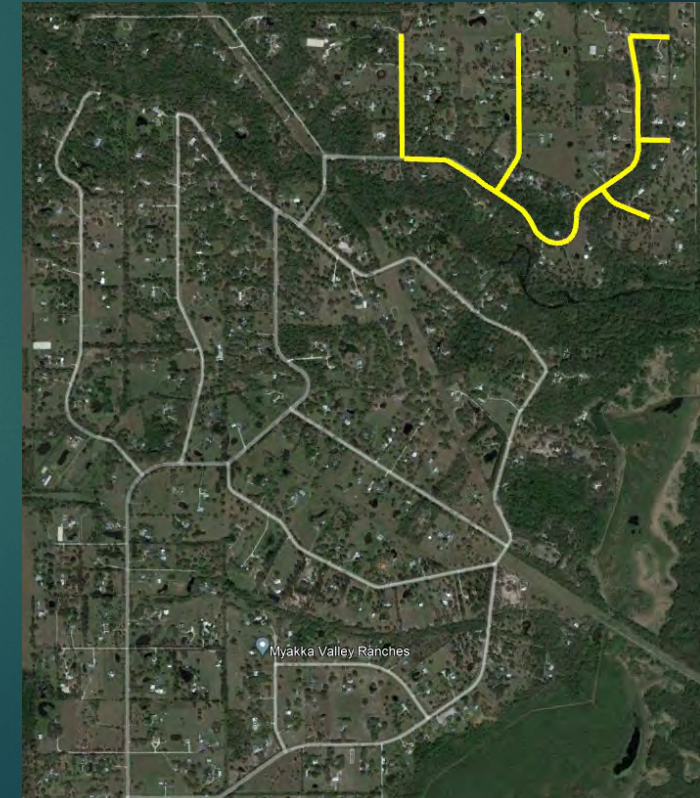


Figure 3: Road Paving Phase 3



Project Prioritization and Approvals

► Project Prioritization

Projects are prioritized based on community priorities such as safety, reliability of critical infrastructure and maintaining property values. The timing of project implementation will depend on available funding, and other community priorities, as voted on by the current board of directors.

► Project Implementation

Project implementation should seek to utilize committee and resident volunteers, where applicable, to reduce costs and help build pride of ownership in community and for projects. Available project funding should be allocated in advance and used to inform project scope to avoid special assessments, wherever possible.

Length (miles)	Road Config.	Drainage Issues	# of Lots served	Condition (1-5)	Last Year Paved	Paving Priority	Paving Priority
3.29	through	no	111	1	2020	low	0
2.57	through	no	75	3	?	high	1
1.26	through	no	23	3	?	high	3
1.15	dead end	yes	28	2	?	medium	4
0.64	through	no	17	2	?	medium	5
0.84	through	no	18	2	?	medium	6
0.45	through	no	7	2	?	medium	8
0.30	through	no	8	2	?	medium	9
0.19	dead end	no	5	2	?	medium	15
0.37	dead end	no	11	2	?	medium	12
0.31	dead end	no	4	2	?	medium	16

► Project Board Approvals and Budgeting

Proposed projects should be developed by committees or board members and presented to the board for discussion and voting.

Proposed projects presented to the board, should include a description of the need for the project, the estimated cost, and other relevant information for the board to decide on the importance and timing of the project.

When available, at least 3 quotes should be obtained from vendors to validate project costs and secure the best value for each project.

For projects requiring a permit, information, costs and process for securing the required permits should be provided.

Upon board approval, previously budgeted funding for the project will be designated for the project, and approved for use, or the required funding will be added to the next years annual budget to support future project implementation.

See Section 9 for more information on funding and financing options.

7.0

DESIGN STANDARDS



- a) Aesthetics
- b) Materials & Specifications



Aesthetics

- ▶ Preservation of the natural look and feel and rural setting is desired to maintain MVR as an equestrian community with a slower pace of life and strong sense of community.
- ▶ Many design elements support this aesthetic quality including the rural road section without curb and gutter, open drainage channels, large parcel size with ample building setbacks and consistent 3-rail wood fencing. Changes to these design elements could adversely affect the country aesthetics.
- ▶ Maintaining these design standards through maintaining and enforcing the current development policies and approval processes will be critical to preserving this desirable aesthetic.



Old Ranch Road



Myakka Valley Trail



Myakka Valley Trail

Materials & Specifications



► Fencing

All property fencing along road easements, shall be wooden 3-rail horizontal fencing on wood posts, located outside of platted easements, with spacing as described in the community deed restrictions. Internal property cross-fencing may be of other materials and designs. Use of alternative fence design or materials along road easements is not permitted.

► Drainage culverts

Driveway drainage culverts shall be a minimum of 15-inch diameter and may be reinforced concrete pipe (RCP) or double walled high-density polyethylene (HDPE) pipe. Corrugated metal pipe (CMP) is not advised due to its rust and crush potential and short service life. Larger diameter culverts may be required in areas with larger drainage conveyance and suggest maintaining or increasing the size of a culvert during replacement, but do not suggest reducing the pipe size. Many residents choose to build end walls for the culverts to help stabilize the ground around the end of the pipe and provide a clean attractive appearance.

Some of the more common culvert pipe materials include:



Round Corrugated
Metal Pipe (CMP)



Round Double wall High
Density Polyethylene (HDPE)



Round Reinforced
Concrete Pipe (RCP)



Elliptical Reinforced
Concrete Pipe (RCP)



Myakka Valley Trail



Old Ranch Road

Materials & Specifications



► Road Signs

Road signs shall be compliant with Sarasota County standards, where applicable, shall meet MUTCD reflectivity, and shall be located no closer than 3 ft from the edge of pavement, with a preference for 5 feet from the edge of pavement, where feasible.

The placement of signs shall not be located within the bottom of roadside drainage swales and shall be placed such that the sign face is easily visible from intended traffic direction, including both face of primary sign and road name signs.

The signposts for stop signs and warning signs shall be 2x2 galvanized metal posts, with bottom of primary road sign at least 6 feet above grade. Signposts for speed limit signs shall be mounted to 4x4x10 ft long pressure treated posts. All posts shall be buried at least 2 ft deep, without concrete base.

The following are preferred sign face dimensions (exceptions may be permitted with board approval):

- Stop sign: 30-inch octagon
- Curve ahead sign (right/left): 24 x 24 inches
- Speed limit sign: 18 x 24 inches
- Object marker sign (diamond): 18 x 18 inches
- Road turns chevron sign: 18 x 24 inches
- Road turns arrow sign: 18 x 36 inches
- Keep right median sign: 24 x 30 inches
- Road name sign: 9 inch tall (width varies)
- 15 mph equestrian sign: 18 x 18 inches
- Watch for wildlife sign: 18 x 18 inches
- Fire Station ahead sign: 30 x 30 inches



Chevron



Object Marker



Fire Station Ahead



Curve Ahead



Road Turns Ahead



Road Turn Speed limit



Road Turns Sign



Keep Right Median



Speed Limit (minor roads)



Speed Limit (major roads)



8.0

EMERGENCY RESPONSE



- a) Storms
- b) Flood
- c) Power outage
- d) Communications outage



Storms



- ▶ Severe weather events have the potential to disrupt our valley by causing power and communications outages, blocking roadway access, and causing damage to our homes, from high winds, hail, lightning, and heavy rainfall causing riverine and localized flooding.
- ▶ Many of these severe weather events occur during hurricane season (June to November, peak season is Aug. to Oct.) and during summertime thunderstorms, and winter cold fronts.
- ▶ **Prior to a storm**, residents should prepare by securing property, trimming trees, filling fuel tanks and preparing for power outage and the possibility of being isolated for a few days.
- ▶ The emergency preparation committee should include public service information via newsletter and other forms of community communications, to remind residents of emergency services and to reach out if assistance is needed.
- ▶ **During a storm**, it is advised to remain in home away from large trees and overhead power lines.
- ▶ **After a storm**, it is recommended to assess damage and notify the board, who will notify the appropriate committees, or authorities to perform repairs, clear roads and drainage systems, and to notify residents to avoid the damaged areas for their safety.
- ▶ If power outages occur, or if power lines are down, residents are encouraged to contact Florida Power and Light (FPL) directly to notify them, and also contact the board.
- ▶ The MVRIA Emergency Preparedness Committee contact info:
 - ▶ Jim Bakowski
 - ▶ Joe DeGilio
 - ▶ Daniel Mendoza
 - ▶ Eddie Oyer



Summer thunderstorm as seen from Old Ranch Road

9.0

PLAN IMPLEMENTATION



- a) Plan Adoption & Updates
- b) Funding
- c) Schedule



Howard Creek, east branch, north of bridge

Plan Adoption & Updates



- ▶ The adoption of this plan will promote consistent application of improvements over time, even with transitions of board and committee members.
- ▶ The plan will also provide transparency for the community regarding the community priorities and needed investments.
- ▶ Through the adoption and use of the plan as a tool to guide board and committee maintenance activities and investment decisions, the plan will instill confidence and help manage limited funding resources for the benefit of our association and our community.

Plan Updates

- ▶ The plan update frequency, is entirely dependent on need, based on changes in community priorities, budgets, or environmental conditions.
- ▶ Review of the plan at least annually is recommended by the board, roads committee, trails committee, and other relevant committees, as needed.
- ▶ Updates to the plan at least every 5-years is recommended.



Funding & Financing



- ▶ The primary source of funding for maintenance and capital projects is from annual dues from MVRIA members.
- ▶ Additional funding may also be collected from property owners outside of the association, but who utilize our roads.
 - ▶ These include Sunfire Nursery, Westwood, and other residential properties outside of the association's platted boundary (toward the front of the neighborhood).
- ▶ Special assessments may be used to cover costs for emergency repairs or other projects deemed necessary by the board of directors. However, these assessments are considered a last resort.



▶ Sarasota County Grants

Sarasota County offers neighborhood and resident focused grants, including the following:

- ▶ [Neighborhood Grant Program](#) that provides up to \$10k in funding for projects that provide:
 - ▶ Community character
 - ▶ Leadership training
 - ▶ Safety
 - ▶ Environmental stewardship
 - ▶ Health education
- ▶ Flood Mitigation Assistance through County to reduce flood risk for individual property owners, via the FEMA flood mitigation program.

▶ Bank Financing

- ▶ When warranted, bank loans may be used to cover important improvements, that cannot wait for the association to accrue needed funds. However, a repayment plan will be needed to ensure this debt does not create an undue burden on the association or members for repayment.

▶ Annual Budget and Financial Statement

- ▶ All paying members can request a copy of our monthly or annual financial statements from PCM (our current management company) at <https://pcmfla.com/>.

10.0

APPENDICES



- a) Land Use – Land Cover
- b) Parcel Map
- c) Trails Map
- d) Subdivision Plat
- e) County/FEMA Flood Maps
- f) Road Signs Map
- g) Drainage Culverts Map
- h) Reference Documents
 - a) MVRIA Guiding Documents
 - b) Sarasota County Regulations (ordinances, zoning)
 - c) Florida Administrative Code (FAC)
 - d) Code of Federal Regulations (CFR)



Land Use – Land Cover



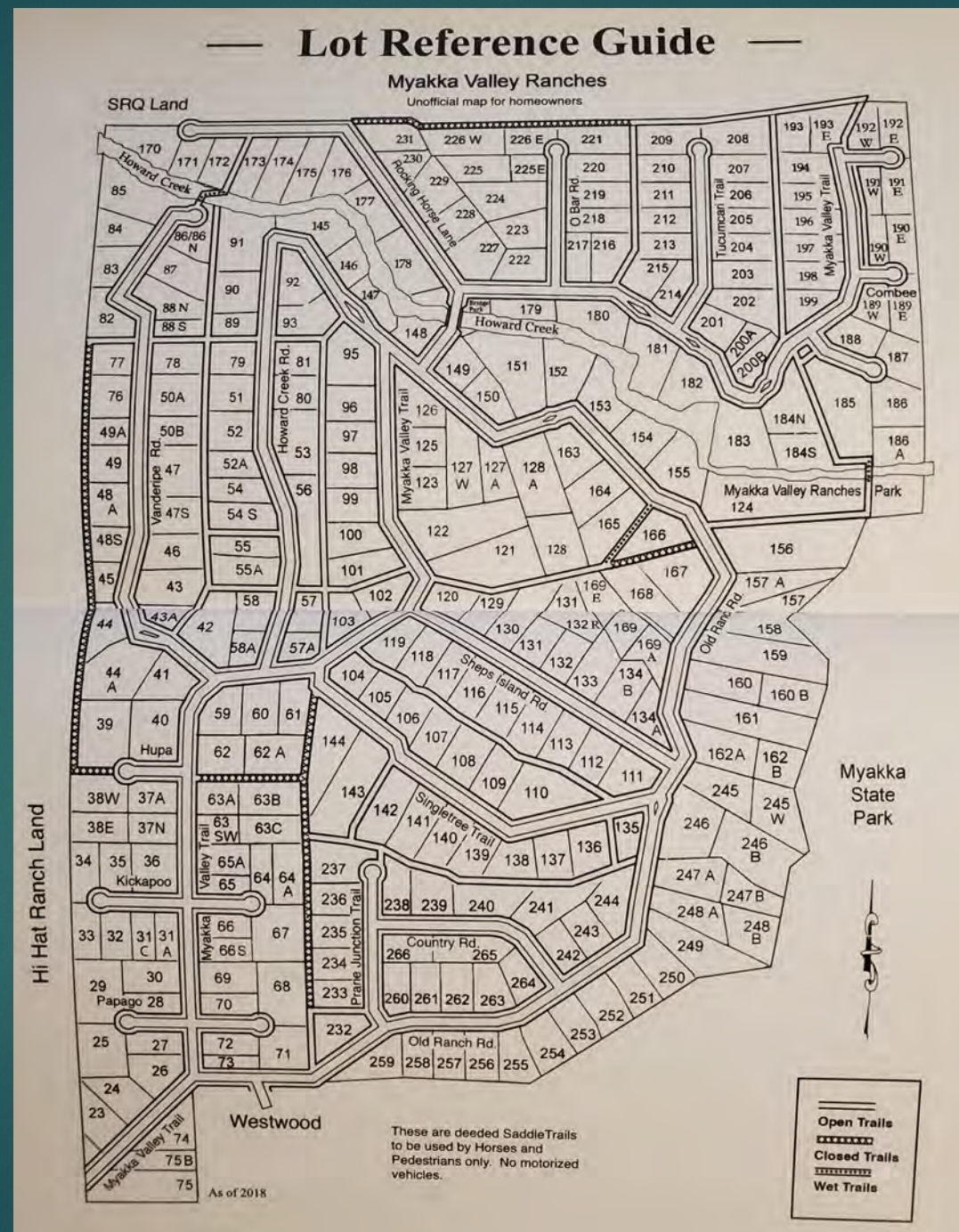
The MVR is comprised of 300± residential parcels of at least 5-acres each, per County zoning requirements. As a private subdivision, most roads, utilities, drainage and equestrian trails are located within dedicated easements across private parcels.

A few tracks are dedicated to the association as public open space (parks) for recreational use by members of the association.

Lot information is available on the Sarasota County property assessor's website.

<https://www.sc-pa.com/propertysearch>

Parcel Map



The MVR is comprised of 300± residential parcels of at least 5-acres each, per County zoning requirements. As a private subdivision, most roads, utilities, drainage and equestrian trails are located within dedicated easements across private parcels.

A few tracks are dedicated to the association as public open space (parks) for recreational use by members of the association.

Lot information is available on the Sarasota County property assessor's website.

<https://www.sc-pa.com/propertysearch>

Trails Map

Trail Legend:

Green: 2.6 miles

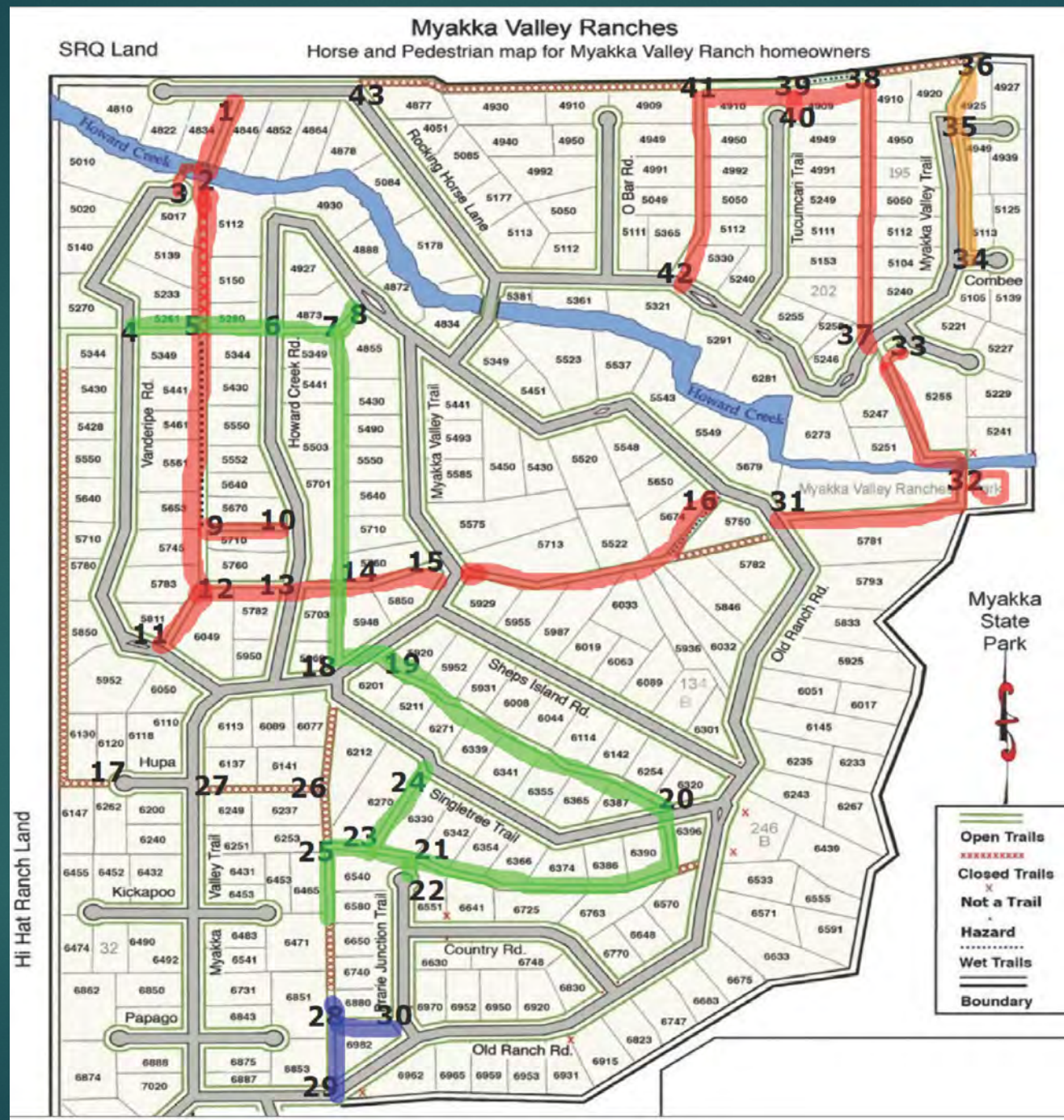
Red: 3.9 miles

Orange: 0.4 miles

Blue: 0.3 miles

TOTAL: 7.2 miles
(open trails)

Approx. 2.5 miles of
closed trails.



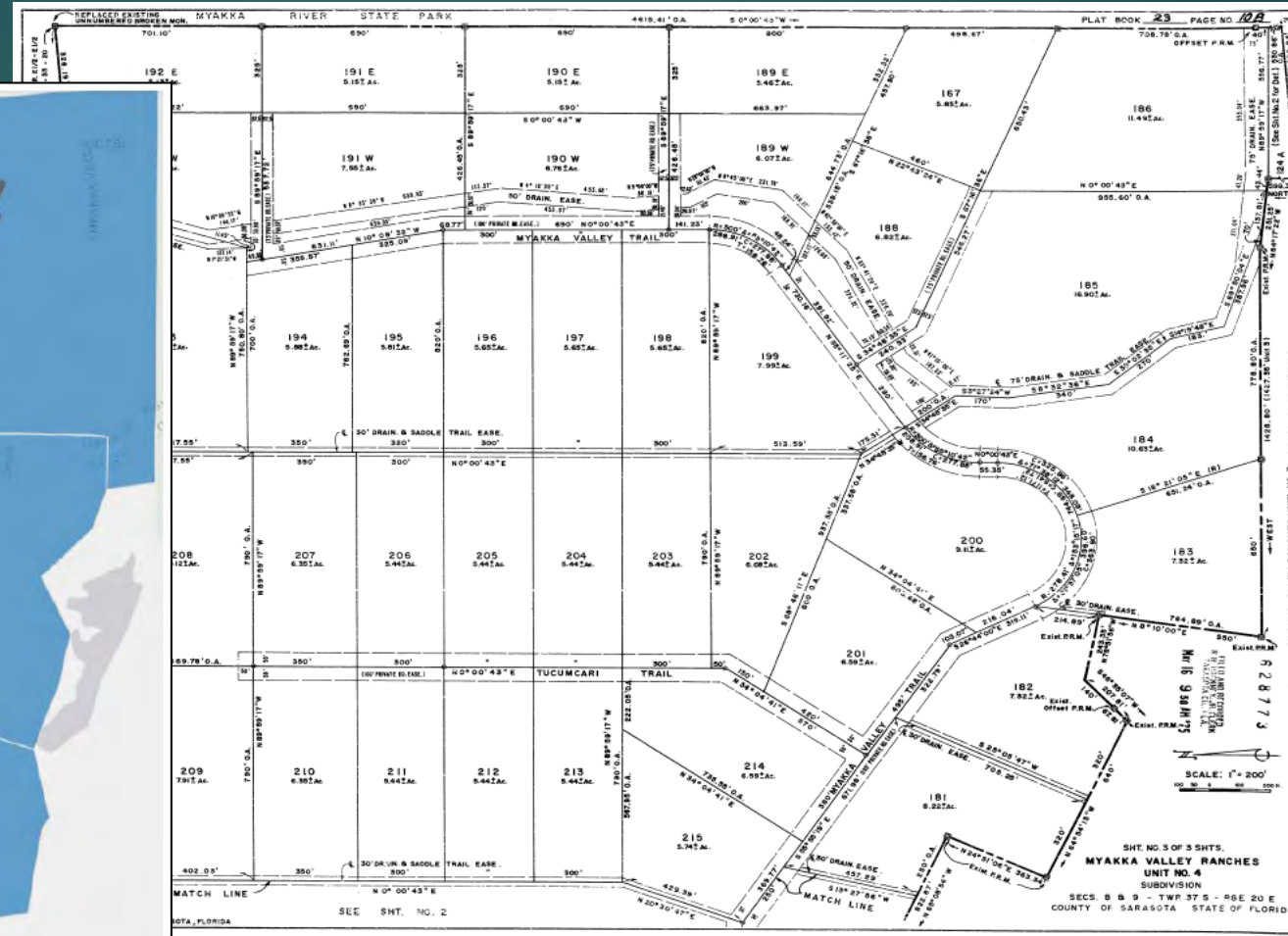
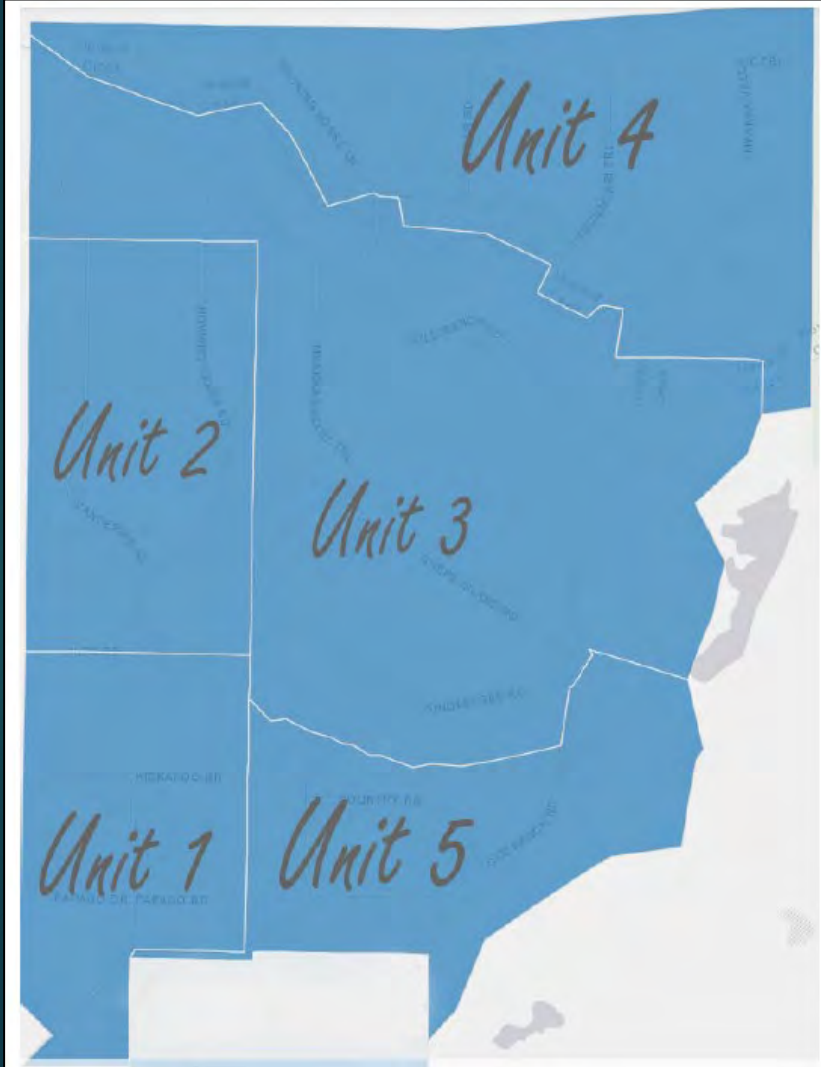
The MVR Trails map is maintained by the Trails Committee, which monitors trail condition and identifies the trails that are accessible, safe and open for use.

While the equestrian trails are designated on the Subdivision Plat, some trails are not open for a variety of reasons including flooding.

The trails map is available on the MVR website.

<https://www.mvria.com/p/Trail-Map>

Subdivision Plat (5 units)

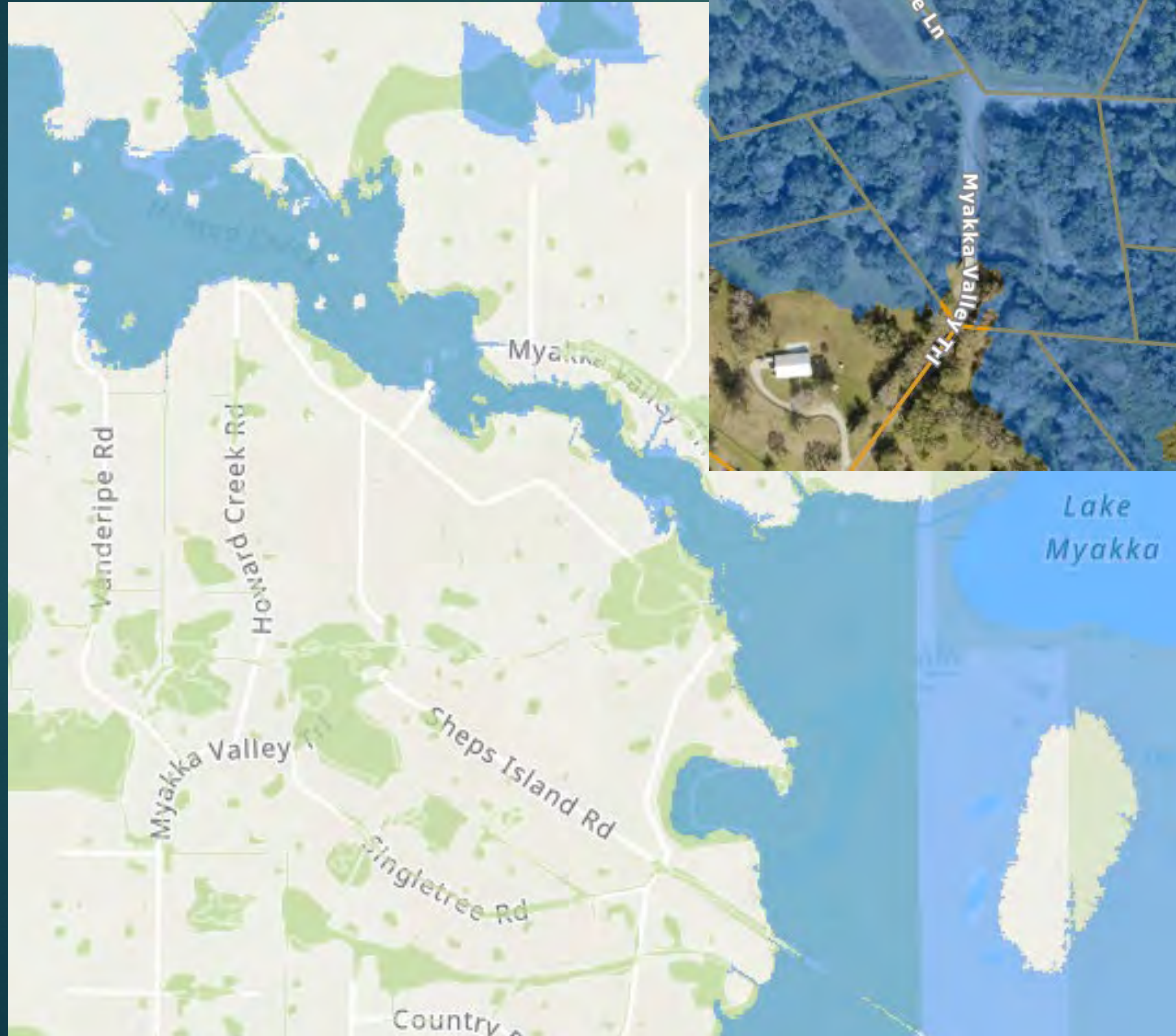


The MVR Subdivision Plat is split into five (5) units recorded between 1969 and 1977. These recorded plats are legal documents that dedicate tracts and easements to the Association for access and maintenance, including roads, drainage, utilities and equestrian trails.

The recorded plats are available on the Sarasota County website.

<https://secure.sarasotaclerk.com/SubPlatsSearch.aspx>

County/FEMA Flood Maps



These Flood Maps are developed by the U.S. Federal Emergency Management Agency (FEMA), known as Flood Insurance Rate Maps (FIRMs), and indicate the extents and water surface elevations of the 100-year (1% annual chance flood) also known as the Base Flood Elevation (BFE) and extents of the 500-year (0.5% annual chance flood) events.

These maps, including preliminary maps, from periodic updates are published by FEMA and Sarasota County.

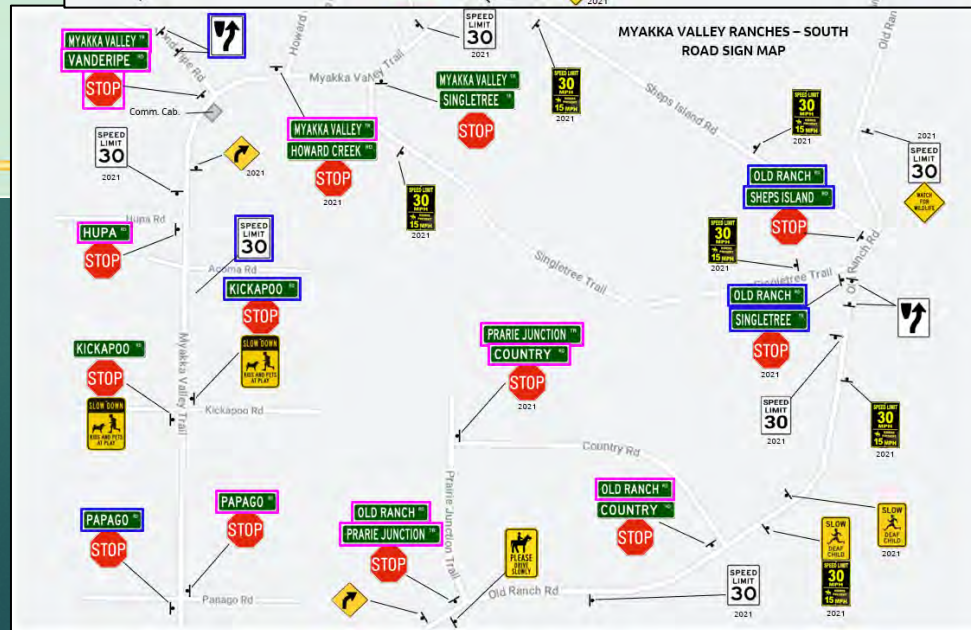
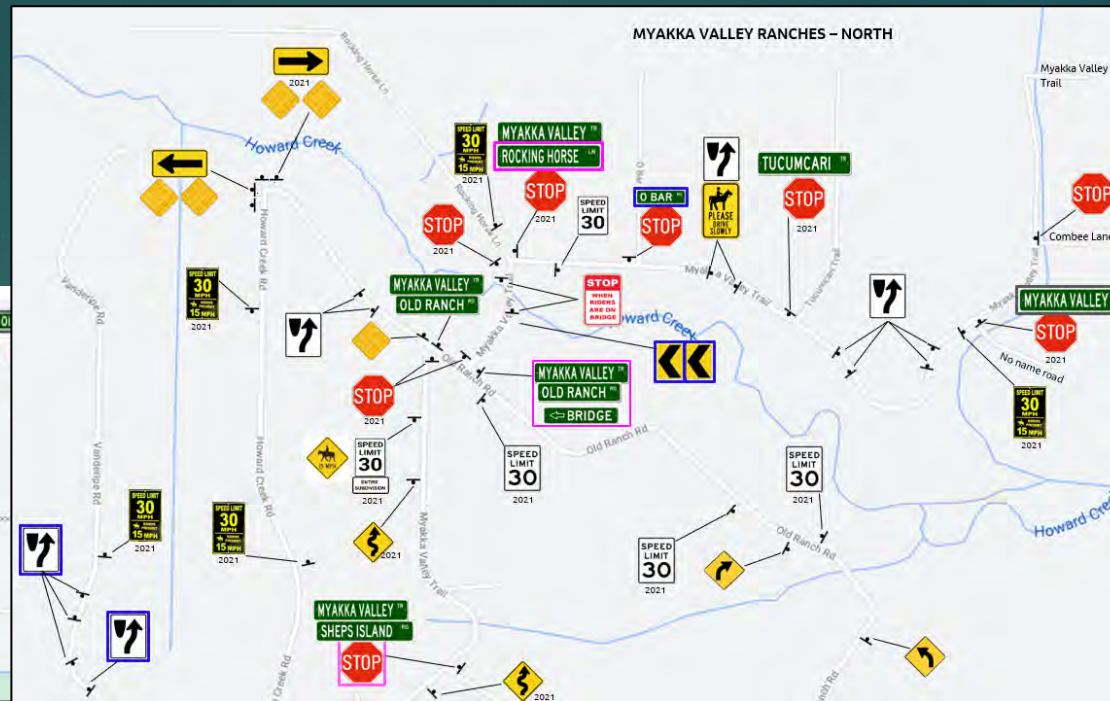
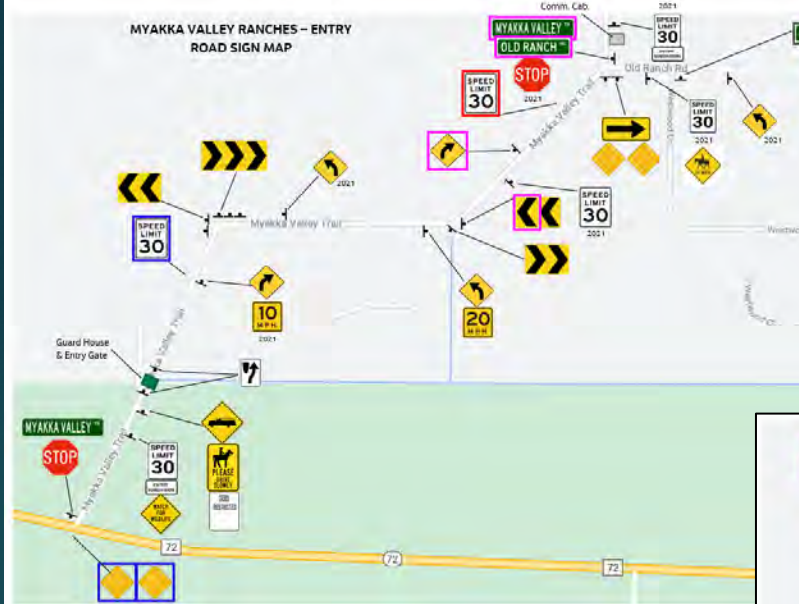
These maps are available on the Sarasota County website, or on FEMA's website.

County Maps:
<https://ags3.scgov.net/sarcoflood/>

FEMA FIRM Maps:
<https://msc.fema.gov/portal/home>

Info:
<https://www.scgov.net/government/public-works/flood-maps>

Road Sign Maps

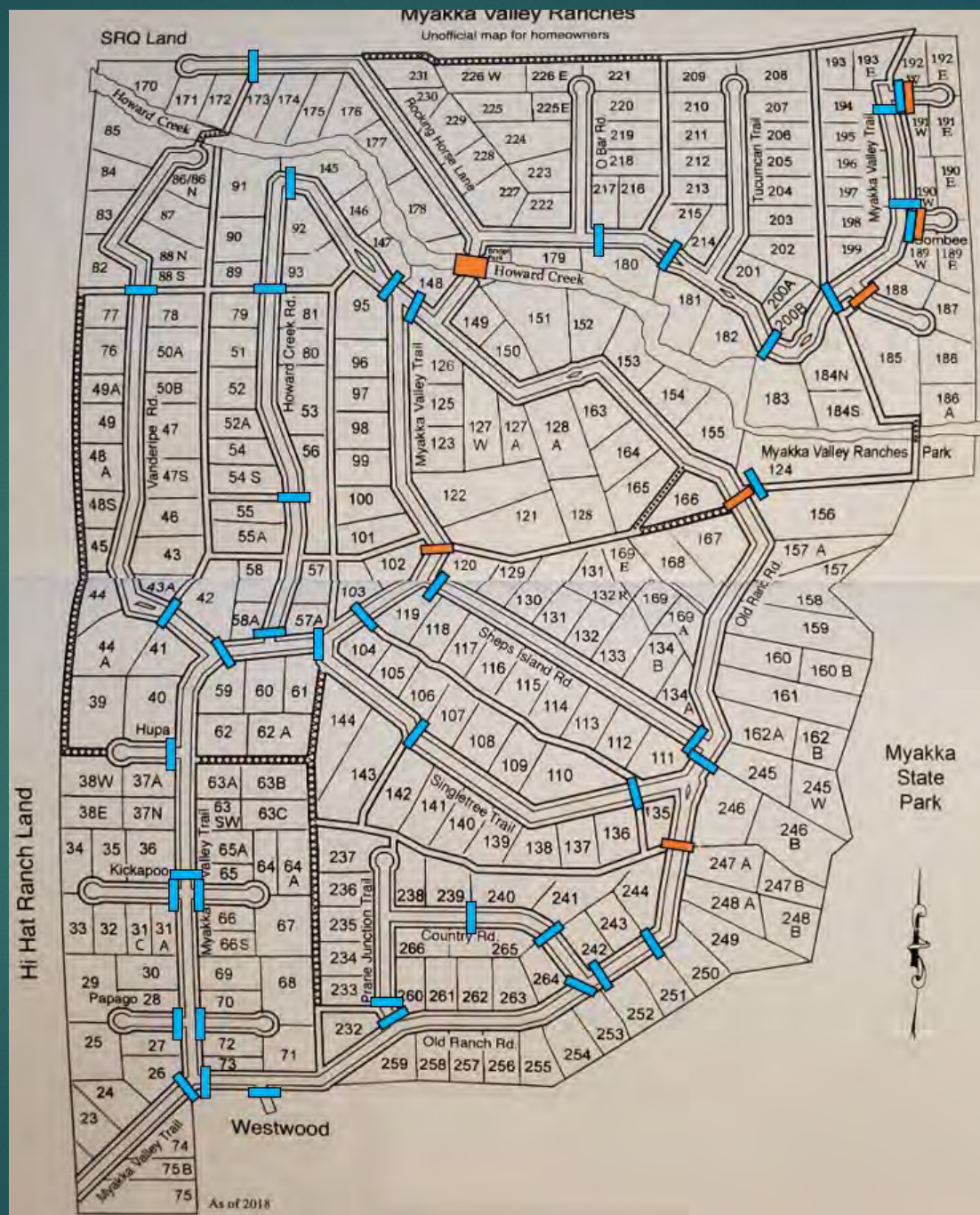


The road sign inventory map was developed by the Roads Committee in 2021 to support road sign condition assessment and replacement, focused on road safety for residents and visitors.

This map will support the continued maintenance and management of road signs to maintain safety and regulatory compliance.

Drainage Culverts

- Approx. 300 private driveway culverts
- Approx. 42 public RCP culverts (under roads)
- Approx. 6 County maintained culverts
- 1 bridge at Howard Creek (west branch)



The map shown here identifies all of the known association and County maintained drainage culverts in MVR.

- ▶ Orange culverts = County maintained culverts & Howard Creek Bridge
- ▶ Blue culverts = MVR Association Responsibility
- ▶ Private driveway easements (not shown) typically 1-2 per lot.



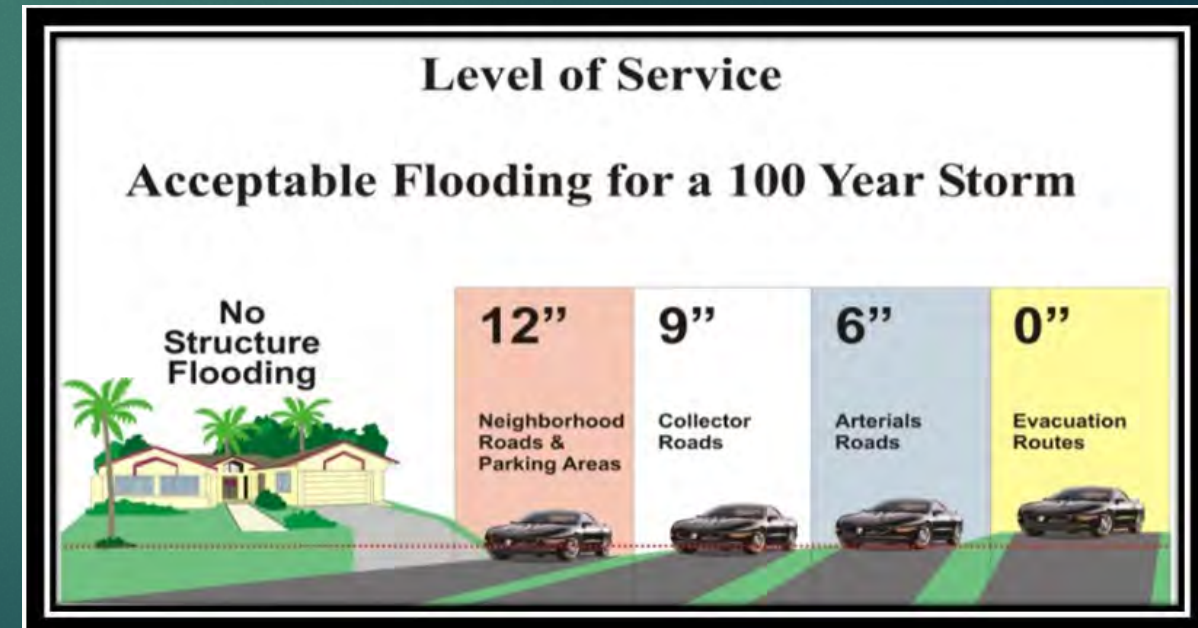
Sarasota County Stormwater Management

► Level of Service (LOS)

- The LOS is a performance rating system for stormwater management and flooding, which uses design storms such as the 5-year, 25-year, or 100-year storm event.
- It is intended to identify improvements required to protect properties from flood damage and minimize flooding of roadways.

► Stormwater Utility Fees

- The fee assessed to all properties in the County are used to pay for stormwater improvements.
- The fee is based on property size, location and other factors.
- More information on the Stormwater Utility, can be found on the County's website: [Stormwater Utility | Sarasota County, FL \(scgov.net\)](http://StormwaterUtility|SarasotaCounty,FL(scgov.net))





We Want to Hear from You

- ▶ Please reach out the Board or Committee members with any inputs for this plan, with questions, concerns, or to report observed hazards in the neighborhood.
 - ▶ Observed hazards or issues may include:
 - ▶ Downed trees or tree branches in the road
 - ▶ Erosion of road shoulders or damaged or missing road signs
- ▶ The board and committee contact information is listed in the Tails and Trails newsletter.
- ▶ Please call 911 to report emergencies.



Thank you